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THE LATEST REVISION OF THIS MANUAL**

Cargo Hook Attach Point Kit

***Kit Part Numbers
200-231-02, 200-231-03***

Owner's Manual

*Owner's Manual Number 120-073-01
Revision 1
May 2, 2012*



*13915 NW 3rd Court Vancouver Washington 98685 USA
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www.OnboardSystems.com*

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RECORD OF REVISIONS

<i>Revision</i>	<i>Date</i>	<i>Page(s)</i>	<i>Reason for Revision</i>
0	02/28/11	All	Initial Release
1	05/02/12	1-3, Section 2, 4-3, 4-4,	Incorporated attach point assembly P/N 232-449-01.

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Section 1

General Information

Introduction

The P/N 200-231-02 and 200-231-03 Cargo Hook Attach Point Kits are intended for installation on the MD Helicopters' 369D, 369E, 369F, 369FF, 369HM, 369HE, 369HS, 500N, and 600N model helicopters. These kits provide the means of attaching a cargo hook to the belly of the helicopter. They use existing holes in the belly and provide the support and pivoting for the cargo hook.

The P/N 200-231-02 Cargo Hook Attach Point Kit is intended for installation on the 369D, 369E, 369F, 369FF, 369HM, 369HE, 369HS, and 500N models and P/N 200-231-03 Cargo Hook Attach Point Kit is for installation on the 600N model. These kits are the same except for the four attach bolts which are of longer grip length in the P/N 200-231-03 kit.

These kits can be used with the Onboard Systems cargo hook kit P/Ns listed in Table 1.1. When installing with each of these kits, the polyurethane bumper on the cargo hook must be removed.

Table 1.1 Compatible Cargo Hook Kits

Kit P/N	STC
200-187-00	SR00407SE
200-264-00	SR00892SE
200-264-01	SR00892SE
200-264-02	SR00892SE
200-300-00	SR01778SE

NOTICE

This Cargo Hook Attach Point Kit is not compatible with MD Helicopters 369H90072 series of cargo hook kits. The MD Helicopters 369H90072 series of cargo hook kits are compatible with Onboard Systems Attach Point Kit P/Ns 200-231-00 and 200-231-01.

Explanation of Signal Words and Symbols

The following definitions apply to the symbols used throughout this manual to draw the reader's attention to safety instructions as well as other important messages.



Indicates a hazardous situation which, if not avoided, will result in death or serious injury.



Indicates a hazardous situation which, if not avoided, could result in death or serious injury.



Indicates a hazardous situation which, if not avoided, could result in minor or moderate injury.



Draws the reader's attention to important or unusual information not directly related to safety.



Used to address practices not related to personal injury.

Bill of Materials

The following items are included with the Attach Point Kits. If shortages are found contact the company from whom the system was purchased.

Table 1.2 Bill of Materials

Part No.	Description	200-231-02 Quantity	200-231-03 Quantity
232-449-01*	Attach Point Assembly	1	1
232-456-00	Spacer Assembly	1	1
510-308-00	Bolt	4	-
510-979-00	Bolt	-	4
510-100-00	Washer	4	4
120-073-01	Owner's Manual	1	1

* Supersedes 232-449-00. 232-449-01 provides compatibility with the Onboard Systems E-51 load cell assembly, otherwise these assemblies are interchangeable (the E-51 load cell assembly is approved under a separate STC).

Inspection

Inspect the kit items for evidence of damage and corrosion. If damage is found, do not use the items until they are repaired.

Specifications

Table 1.3 Specifications

Design Load*	2,500 lbs (1134 kgs)
Design Ultimate Strength	9,375 lbs (4,252 kgs)



* The design load is an indication of the structural capacity of the Cargo Hook Attach Point. All helicopter external load limits as described in the RFM still apply.

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Section 2

Installation Instructions

These procedures are provided for the benefit of experienced aircraft maintenance facilities capable of carrying out the procedures. They must not be attempted by those lacking the necessary expertise.

Installation

If present, remove existing cargo hook attach point components from the belly of the helicopter.

Apply sealant to surfaces shown in Figure 2.1 and attach the Onboard Systems Attach Point Assembly (P/N 232-449-01) and Spacer Assembly (P/N 232-456-00) using the supplied bolts (P/N 510-308-00) and washers (P/N 510-100-00), see Figure 2.1. If necessary clear sealant from the two small Ø.096” drain holes in Spacer Assembly block.

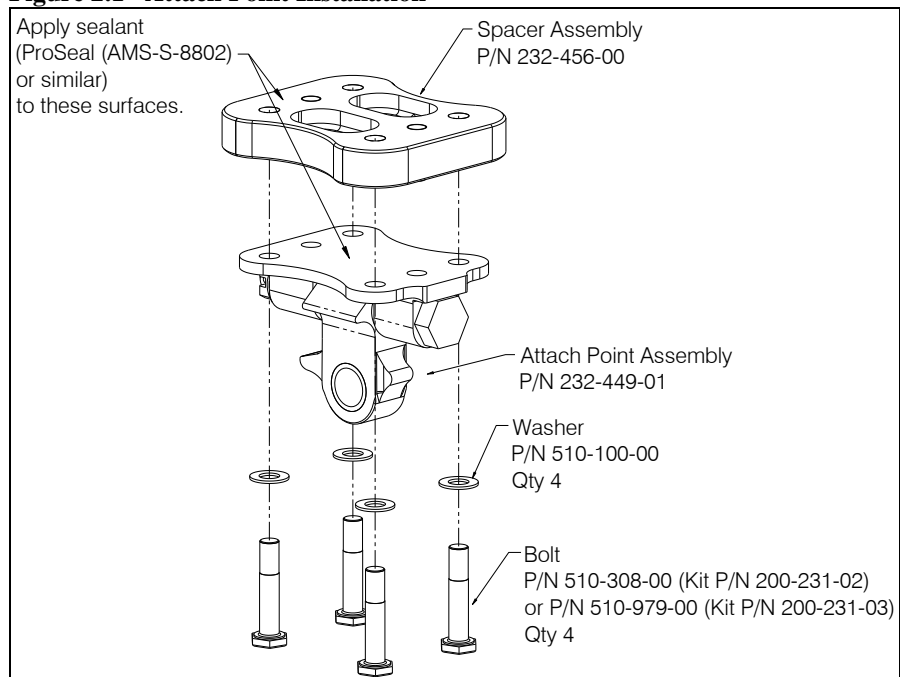


Orientation of the Attach Point Assembly is dependent on the cargo hook kit which this attach point kit is to be used with, see Figure 2.2 and 2.3.

Torque bolts to 50-80 in-lbs and safety wire.

Since the Attach Point Assembly has built in travel limiters, the rubber pads (if present) glued to the skin can be removed at the installer's option.

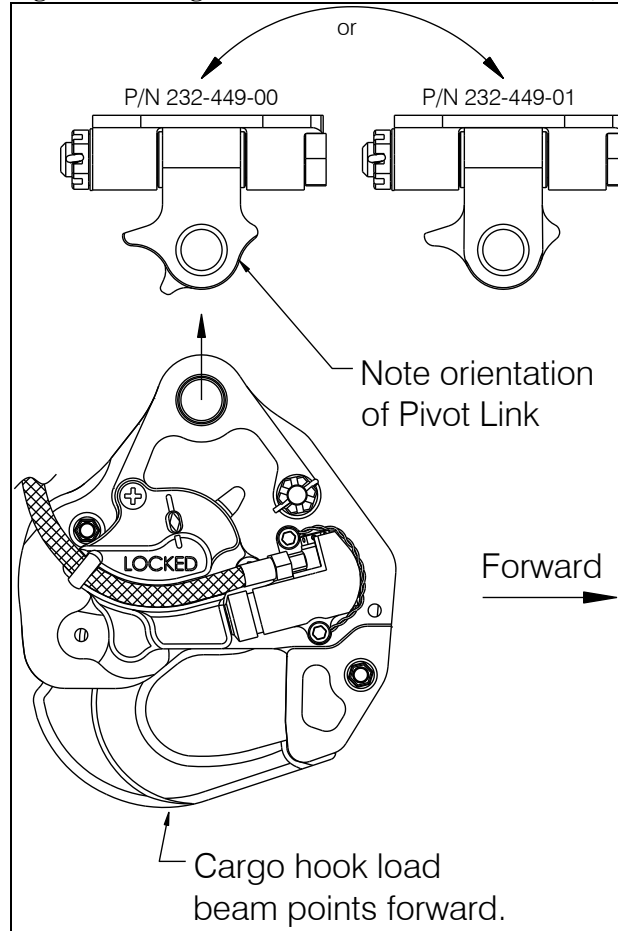
Figure 2.1 Attach Point Installation



Installation continued

If the attach point kit is to be used with Kit P/N 200-300-00 orient the attach point as shown in Figure 2.2.

Figure 2.2 Cargo Hook/Attach Point Orientation (P/N 200-300-00)



If the attach point kit is to be used with any of the following kits listed in the table below reverse the pivot link orientation by turning the attach point assembly 180 degrees or removing the pivot link and installing it the opposite direction (see Figure 2.3).

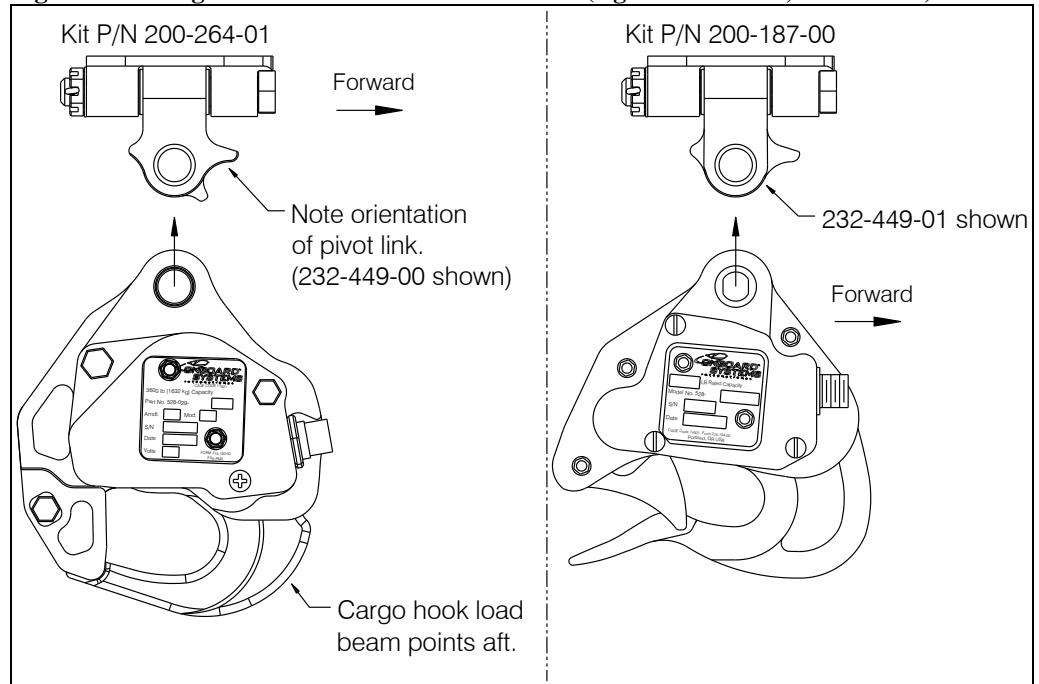
Kit P/N	Cargo Hook P/N
200-187-00	528-010-04
200-264-00	528-023-01
200-264-01	528-029-00
200-264-02	528-029-00

NOTICE

If installing the attach point kit with any of the kits listed above, the polyurethane cargo hook bumper is not used and must be removed.

Installation continued

Figure 2.3 Cargo Hook/Attach Point Orientation (e.g. – 200-264-01, 200-187-00)



Installation Check-out

After installing the attach point kit, perform the following functional checks.

1. Swing the pivot link throughout its range of motion and ensure it pivots freely.
2. If installed, swing the Cargo Hook to ensure that the manual release cable assembly (or hydraulic hose if Onboard Systems cargo hook P/N 528-028-00 is installed) and the electrical release harness have enough slack to allow full movement of the attach point assembly without straining or damaging them. The manual release system or electrical harness must not be the stops that prevent the Cargo Hook from swinging freely in all directions.

Weight

Table 2.1 Component Weights

Item	Weight
Spacer Assembly (P/N 232-456-00)	.33 lbs (.15 kgs)
Attach Point Assembly (P/N 232-449-01)	.81 lbs (.37 kgs)
Attach Point hardware	.09 lbs (.04 kgs)
Total Kit Weight	1.23 lbs (.56 kgs)

Paper Work

In the US, fill in FAA form 337 for the initial installation. This procedure may vary in different countries. Make the appropriate aircraft log book entry.

Section 3

Operation Instructions

Operating Procedures

Prior to a flight involving external load operations perform the following:

1. Ensure that the attach point hardware is secure and that the manual release system and electrical release harness do not limit the movement of the Attach Point or cargo hook.
2. Ensure that the attach point's pivot points freely rotate and that there is no binding.

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Section 4

Maintenance

Storage Instructions

Clean the components thoroughly before packaging. Pack the unit in a heat-sealable package. If the unit is to be stored for long periods in a tropical climate it should be packed in a reliable manner to suit local conditions. Refer to relevant MIL specifications.

Package the unit in a suitable fiberboard box and cushion the unit to prevent shifting. Seal the fiberboard box with tape and mark the box with the contents and date of packaging.

Preventive Maintenance

Remove caked-on dirt from the components with a brush and clean exposed surfaces with a mild solvent. Thoroughly dry all surfaces.

Annually, it is recommended that the Attach Point be disassembled and grease applied (Mobilgrease 28 or similar) to the outside of each pivot point bolt.

Daily Check

Prior to a flight involving external load work, perform the following checks of the attach point kit.

1. Move the cargo hook and attach point throughout their full ranges of motion and observe the manual release system and electrical harnesses. The manual release system and electrical harnesses must not be the stops that prevent the cargo hook or attach point from pivoting freely in all directions.
2. Ensure the pivot points, including the pivot link within the cradle and the cargo hook pivot point, rotate freely.
3. Visually check for presence and security of the four bolts which secure the attach point to the helicopter.
4. Visually check the Pivot Link and Pillow Block for cracks and damage.

The scheduled inspection interval(s) noted below are maximums and are not to be exceeded. If the attach point kit is subjected to unusual circumstances, extreme environmental conditions, etc., it is the responsibility of the operator to perform the inspections more frequently to ensure proper operation.

Inspection

Annually or 100 hours of external load operations, whichever comes first, inspect the attach point kit components per the following.



*Hours of external load operations should be interpreted to be when (1) anything is attached to the primary cargo hook (whether or not a useful load is being transported) and (2) the aircraft is flying. If these conditions are **NOT** met, time does **NOT** need to be tracked.*

1. Move the cargo hook and attach point throughout their full ranges of motion and observe the manual release system and electrical harnesses. The manual release cable or hydraulic hose and electrical harnesses must not be the stops that prevent the cargo hook or attach point from pivoting freely in all directions.
2. Ensure the pivot points, including the pivot link within the Pillow Block and the cargo hook pivot point, rotate freely.
3. Visually inspect for presence and security of the four bolts which secure the attach point to the helicopter.
4. Visually inspect the Pivot Link and Pillow Block for cracks and damage.

Inspection continued

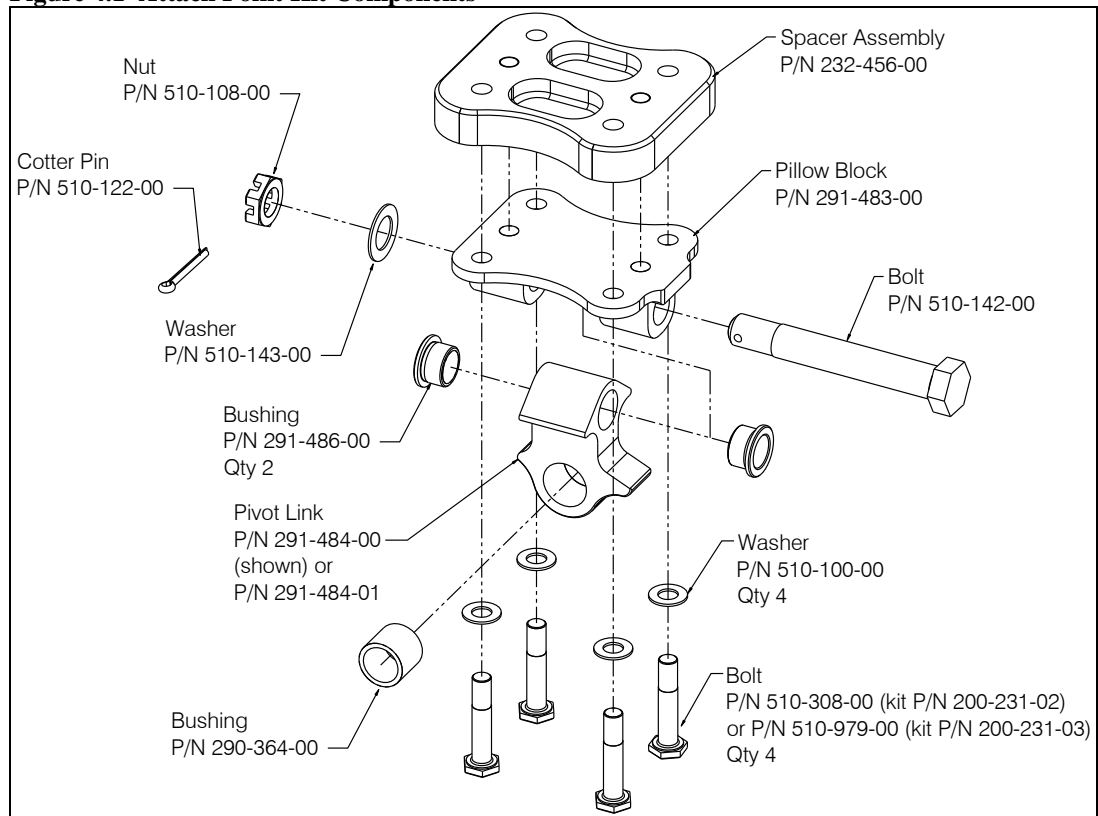
Every 1000 hours of external load operations or 5 years, whichever comes first, remove the attach point kit components from the helicopter and inspect per the following.

Carefully inspect, and if necessary repair, the detail parts in accordance with the instructions in Table 4.1. Refer to Figure 4.1 for part identification. Inspect the parts in a clean, well-lit room.

Perform magnetic particle inspection in accordance with ASTM-E1444 and MIL-STD-1907, Grade A on the parts listed below. No cracks are permitted in any of these parts.

1. Pillow Block (P/N 291-483-00), Qty 1
2. Pivot Link (P/N 291-484-00 or P/N 291-484-01), Qty 1

Figure 4.1 Attach Point Kit Components



For re-assembly after inspection, tighten the nut (P/N 510-108-00) to finger tight, then rotate to next castellation to install the cotter pin (P/N 510-122-00).

If bushings are replaced, press in with wet zinc chromate primer (TT-P-1757 or equivalent).

Inspection continued

Table 4.1 Attach Point Kit Inspection Criteria

Component	Damage Permitted without Repair	Repair	Maximum Damage which Causes Replacement
Pivot Link P/N 291-484-00 or P/N 291-484-01*	Dents, nicks, gouges, scratches, and corrosion less than .005" (.13 mm) deep.	Blend at 20:1 ratio, length to depth, to provide smooth transitions. Part is 15-5 stainless steel, no touch-up finish is required.	Dents, nicks, gouges and scratches greater than .020" (.51 mm) deep. Cracks.
Pillow Block P/N 291-483-00	Dents, gouges, scratches, and corrosion less than .005" (.13 mm) deep.	Blend at 20:1 ratio, length to depth, to provide smooth transitions.	Dents, gouges and scratches greater than .020" (.51 mm) deep. Cracks.
Bushing P/N 291-486-00	Wear on inside diameter, diameter less than or equal to .450" (11.43 mm).	None.	Wear on inside diameter, diameter greater than .450" (11.43 mm)
Bushing P/N 290-364-00	Wear on inside diameter, diameter less than or equal to .520" (13.21 mm).	None.	Wear on inside diameter, diameter greater than .520" (13.21 mm).
Spacer Block P/N 291-485-00	Dents, nicks, gouges, scratches, and corrosion less than .020" (.51 mm) deep.	Blend at 20:1 ratio, length to depth, to provide smooth transitions. Touch up with zinc chromate primer.	Dents, nicks, gouges and scratches greater than .040" (1.02 mm) deep. Cracks.
Bolt P/N 510-142-00	Wear on outside diameter, diameter greater than or equal to .420" (10.74 mm).	None.	Wear on outside diameter, diameter less than .420" (10.74 mm).

* P/N 291-484-01 (part of attach point assembly 232-449-01, reference Figure 2.2) supersedes P/N 291-484-00 (part of attach point assembly 232-449-00). It provides compatibility with the Onboard Systems E-51 load weigh system. Otherwise these parts are interchangeable.

Instructions for Returning Equipment to the Factory

If an Onboard Systems product must be returned to the factory for any reason (including returns, service, repairs, overhaul, etc) obtain an RMA number before shipping your return.



An RMA number is required for all equipment returns.

- To obtain an RMA, please use one of the listed methods.
 - Contact Technical Support by phone or e-mail (Techhelp@OnboardSystems.com).
 - Generate an RMA number at our website: <http://www.onboardsystems.com/rma.php>
- After you have obtained the RMA number, please be sure to:
 - Package the component carefully to ensure safe transit.
 - Write the RMA number on the outside of the box or on the mailing label.
 - Include the RMA number and reason for the return on your purchase or work order.
 - Include your name, address, phone and fax number and email (as applicable).
 - Return the components freight, cartage, insurance and customs prepaid to:
Onboard Systems
13915 NW 3rd Court
Vancouver, Washington 98685
USA
Phone: 360-546-3072

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Section 5 Certification

FAA STC

United States of America
Department of Transportation—Federal Aviation Administration

Supplemental Type Certificate

Number SR00525SE

This certificate, issued to **Onboard Systems
13915 NW 3rd Court
Vancouver, WA 98685**

certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 6 of the Civil Air Regulations.

Original Product—Type Certificate Number: H3WE
Make: MD Helicopters Inc. (MDHI)
Model: 369D, 369E, 369F, 369FF, 369HE, 369HM, 369HS, 500N, and 600N

Description of the Type Design Change: Fabrication of Onboard Systems Model 200-231-00, 200-231-01, 200-231-02, and 200-231-03 Cargo Hook Attach Point Kits in accordance with FAA Approved Onboard Systems Master Drawing List No. 155-038-00, rev. 9, dated March 9, 2011, or later FAA approved revision.

Attach Point Kit Model No.	Install per Onboard Systems Owner's Manual	Inspect and Maintain per Onboard Systems Owner's Manual
200-231-00 or 200-231-01	Doc. No. 120-073-00, rev. 9, dated March 9, 2010	Sect. 4 of Doc. No. 120-073-00, rev. 9, dated March 9, 2010
200-231-02 or 200-231-03	Doc. No. 120-073-01, rev. 0, dated February 28, 2011	Sect. 4 of Doc. No. 120-073-01, rev. 0, dated February 28, 2011


Limitations and Conditions: Approval of this change in type design applies to only those McDonnell Douglas model rotorcraft listed above. This approval should not be extended to rotorcraft of these models on which other previously approved modifications are incorporated unless it is determined by the installer that the relationship between this change and any of those other previously approved modifications, including changes in type design, will introduce no adverse effect upon the airworthiness of that rotorcraft. No Rotorcraft Flight Manual revision is deemed necessary for this Cargo Hook Attach Point Kit installation. A copy of this Certificate must be maintained as part of the permanent records of the modified rotorcraft.


If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked, or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.

Date of application: May 12, 1998
Date of issuance: June 22, 1998

Date reissued:
Date amended: 1/13/2003, 10/8/2008, 8/4/2011

By direction of the Administrator

(Signature)
Acting Manager, Seattle Aircraft Certification Office
(Title)



Any alteration of this certificate is punishable by a fine of not exceeding \$1,000, or imprisonment not exceeding 3 years, or both.

This certificate may be transferred in accordance with FAR 21.47.

FAA FORM 8110-2(10-68)



Supplemental Type Certificate

This approval is issued to:

Onboard Systems
13915 North West 3rd Court
Vancouver, Washington
United States of America 98685

Number: SH98-47

Issue No.: 3

Approval Date: September 14, 1998

Issue Date: November 15, 2011

Responsible Office:

Pacific

Aircraft/Engine Type or Model:

McDonnell Douglas HC
369D, E, F, FF, HE, HM, HS, 500N, and 600N

Canadian Type Certificate or Equivalent:

H3WE (369 Series), H-95 (500N, 600N)

Description of Type Design Change:

Fabrication and Installation of Onboard Systems Cargo Hook Attach Point per FAA STC SR00525SE

Installation/Operating Data,

Required Equipment and Limitations:

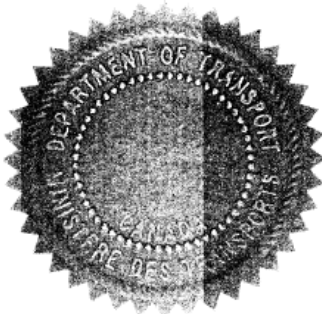
Fabrication of Onboard Systems Model 200-231-00, 200-231-01, 200-231-02 and 200-231-03 Cargo Hook Attach Point in accordance with FAA approved Onboard Systems Master Drawing List No. 155-038-00, Rev. 9, dated March 9, 2011 *; and,

Installation of 200-231-00 or 200-231-01 Cargo Hook Attach Point Kit in accordance with FAA approved Onboard Systems Owner's Manual No. 120-073-00, Rev. 9, dated March 9, 2010 *; or of 200-231-02 or 200-231-03 Cargo Hook Attach Point Kit in accordance with FAA approved Onboard Systems Owner's Manual No. 120-073-01, Rev. 0, dated February 28, 2011 *.


Inspect and Maintain Cargo Hook Attach Point Kit in accordance with Section 4 of Onboard Systems Owner's Manual No. 120-073-00, Rev. 9, dated March 9, 2011 * for Onboard Systems Model 200-231-00 and 200-231-01; or Owner's Manual No. 120-073-01, Rev. 0, dated February 28, 2011 * for Onboard Systems Model 200-231-02 and 200-231-03.

(* or later FAA approved revisions)

- End -



Conditions: This approval is only applicable to the type/model of aeronautical product specified therein. Prior to incorporating this modification, the installer shall establish that the interrelationship between this change and any other modification(s) incorporated **will not** adversely affect the airworthiness of the modified product.


H. W. Wong
For Minister of Transport



SUPPLEMENTAL TYPE CERTIFICATE

10043044, REV. 1

This Supplemental Type Certificate is issued by EASA, acting in accordance with Regulation (EC) No. 216/2008 on behalf of the European Community, its Member States and of the European third countries that participate in the activities of EASA under Article 66 of that Regulation and in accordance with Commission Regulation (EU) No. 748/2012 to

ONBOARD SYSTEMS INTERNATIONAL

**13915 NW 3RD COURT
VANCOUVER WA 98685
USA**

and certifies that the change in the type design for the product listed below with the limitations and conditions specified meets the applicable Type Certification Basis and environmental protection requirements when operated within the conditions and limitations specified below:

Original Type Certificate Number : FAA H3WE
Type Certificate Holder : MD HELICOPTERS INC
Type Design - Model : 369D, 369E, 369F, 369FF
369HE, 369HM, 369HS
500N, 600N
Original STC Number : FAA STC SR00525SE

Description of Design Change:
Installation of Onboard Systems Model 200-231-00, 200-231-01, 200-231-02 and 200-231-03 Cargo Hook Attachment Point Kits in accordance with Onboard Systems Master Drawing List No. 155-038-00, Rev. 11 dated October 26, 2012

EASA Certification Basis:
The Certification Basis (CB) for the original product remains applicable to this certificate/ approval. The requirements for environmental protection and the associated certified noise and/ or emissions levels of the original product are unchanged and remain applicable to this certificate/ approval.

See Continuation Sheet(s)

For the European Aviation Safety Agency,

Date of issue: 08 June 2009

for 
Massimo MAZZOLETTI
Head of Rotorcraft Department

Note:
The following numbers are listed on the certificate:
EASA old Project Number: P-EASA IM R S 01530, REV. 1

SUPPLEMENTAL TYPE CERTIFICATE - 10043044, REV. 1 - ONBOARD SYSTEMS INTERNATIONAL

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Associated Technical Documentation:

- Installation: Installation of the 200-231-00 and 200-231-01 Cargo Hook Attachment Point Kits must be done in accordance with FAA approved Onboard Systems Owner's Manual No. 120-073-00 Rev.7 dated October 20, 2010 and installation of the 200-231-02 and 200-231-03 Cargo Hook Attachment Point Kits must be done in accordance with FAA approved Onboard Systems Owner's Manual No. 120-073-01 Rev.1 dated May 2, 2012.
- Inspection: Inspection of the Cargo Hook Attachment Point Kits should be done in accordance with Section 4 of Onboard Systems Owner's Manual No. 120-073-00 Rev.7 dated October 20, 2010 for Models 200-231-00 and 200-231-01 and Owner's Manual No. 120-073-01, Rev. 1 dated May 2, 2012 for Models 200-231-02 and 200-231-03.

Limitations/Conditions:

Prior to installation of this design change it must be determined that the interrelationship between this design change and any other previously installed design change and/ or repair will introduce no adverse effect upon the airworthiness of the product.

- end -

Note:
The following numbers are listed on the certificate:
EASA old Project Number: P-EASA.IM.R.S.01530, REV. 1

SUPPLEMENTAL TYPE CERTIFICATE - 10043044, REV. 1 - ONBOARD SYSTEMS INTERNATIONAL

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DCA Malaysia STC (600N only)



JABATAN PENERBANGAN AWAM MALAYSIA
(DEPARTMENT OF CIVIL AVIATION MALAYSIA)
ARAS 1-4, BLOK PODIUM
NO. 27, PERSIARAN PERDANA, PRESINT 4
PUSAT PENTADBIRAN KERAJAAN PERSEKUTUAN
62618 PUTRAJAYA
MALAYSIA

Telefon : 6-03-88714000
Faks : 6-03-88901640
AFTN : WMGKYAYX
Teleks : PENAMA MA 30128
Kawat : CIVIL KUALA LUMPUR
Web : <http://www.dca.gov.my>



Your Ref. :
Our Ref. : DCA/AW/VSTC/2017/004
Date : *7 March 2017*

Onboard Systems
13915 NW 3rd Court
Vancouver, WA 98685

Attn: **Mr. Mark Hanson**
Certification Manager

Dear Sir,

VALIDATION OF FAA SUPPLEMENTAL TYPE CERTIFICATE NO. SR00525SE

This is in reference to FAA letter 102S-16-256 dated Nov 4, 2016 and your letter to the FAA dated October 7, 2016 requesting validation of FAA Supplemental Type Certificate (STC) No. SR00525SE date amended 4 August 2011.

This letter certifies that the following Supplemental Type Certificate (STC) is acceptable for Malaysian Airworthiness Certification:-

STC Validation Reference No:	VSTC/2017/004
STC Holder	Onboard Systems, 13915 NW 3 rd Court, Vancouver, WA 98685
STC Reference Number	SR00525SE date amended 4 August 2011 or later FAA approved revision.
State of Design	FAA
Description of Design Change	Fabrication and Installation of Onboard System Model 200-231-03 Cargo Hook Attach Point Kit
STC applicable to Aircraft/ Engine type or model	MD Helicopters Inc. (MDHI) 600N
DCA Type Acceptance	Type acceptance letter reference DCA/AW/AT/MD600N dated 19 December 2016

Page 1 of 2

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DCA Malaysia STC (600N only) continued

Limitations and Conditions:-

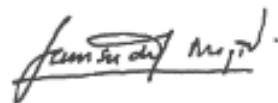
1. Fabrication of Onboard Systems Model 200-231-03 Cargo Hook Attach Point Kit in accordance with FAA Approved Onboard Systems Master Drawing List No. 155-038-00, rev. 9, dated March 9, 2011, or later FAA approved revision.
2. Installation of Onboard Systems Model 200-231-03 Cargo Hook Attach Point Kit in accordance with FAA Approved Onboard Systems Owner's Manual Doc. No. 120-073-01, rev. 0, dated February 28, 2011, or later FAA approved revision.
3. This modification must be inspected and maintained in accordance with Section 4 of FAA Approved Onboard Systems Owner's Manual Doc. No. 120-073-01, rev. 0, dated February 28, 2011, or later FAA approved revision.
4. Approval of this change in type design applies to only those McDonnell Douglas Helicopter model rotorcraft listed above.
5. This approval should not be extended to rotorcraft of these models on which other previously approved modifications are incorporated unless it is determined by the installer that the relationship between this change and any of those other previously approved modifications, including changes in type design, will introduce no adverse effect upon the airworthiness of that rotorcraft.
6. No Rotorcraft Flight Manual revision is deemed necessary for this Cargo Hook Attach Point Kit installation.
7. A copy of STC SR00525SE must be maintained as part of the permanent record for the modified aircraft.
8. If the holder agrees to permit another person to use STC SR00525SE to alter the product, the holder shall give the other person written evidence of that permission.
9. Appropriate installation approval shall be obtained for the embodiment of STC SR00525SE.
10. All other limitations and conditions as stated in STC SR00525SE.

This validation letter shall remain valid unless otherwise cancelled, superseded or revoked by the Director General of Civil Aviation.

Thank You

"BERKHIDMAT UNTUK NEGARA"

Yours faithfully,



(SAMSUDIN AB MAJID)
Airworthiness Sector
For Director General Civil Aviation
MALAYSIA