

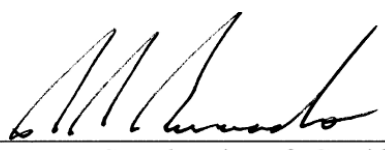
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FAA APPROVED

**ROTORCRAFT FLIGHT MANUAL
SUPPLEMENT**

**Bell Models
206L, 206L-1, 206L-3, 206L-4, 407**

R/N _____ S/N _____

FAA Approved: 
Fol Manager, Seattle Aircraft Certification Office


Date: 11 DEC 14
Revised:

	Rotorcraft Flight Manual Supplement	Document Number 121-002-00	
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INTRODUCTION

This supplement must be attached to the appropriate Bell FAA approved Rotorcraft Flight Manual when an Onboard Systems 200-260-00 Cargo Hook Kit is installed in accordance with Supplemental Type Certificate (STC) NO. SR00850SE. The information contained herein supplements or supersedes the basic manual only in those areas listed herein. For limitations, procedures and performance information not contained in this supplement, consult the basic Rotorcraft Flight Manual and Rotorcraft Flight Manual Supplement-Cargo Hook issued by Bell.

The 200-260-00 Cargo Hook Kit serves as an upgrade kit for the factory installed cargo hook and interfaces with the helicopter's existing cargo hook provisions including the suspension beam assembly on the belly.

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1. LIMITATIONS

1-3. Types of Operation

The basic Rotorcraft Flight Manual and Rotorcraft Flight Manual Supplement – Cargo Hook issued by Bell remain applicable and are complemented by the following.

With a load attached to the cargo hook, operation shall be conducted in accordance with the respective national operational requirements. For US operators 14 CFR part 133 is applicable.

The cargo hook kit configuration (as installed in accordance with this STC SR00850SE) does not meet the 14 CFR part 27 certification requirements for Human External Cargo (HEC).



The cargo hook equipment certification approval does not constitute operational approval; operational approval for external load operations must be granted by the local Aviation Authority.

1-6. Weight and Center of Gravity

Consult the Rotorcraft Flight Manual Supplement – Cargo Hook issued by Bell for Weight and Center of Gravity Limitations.

The maximum weight to be carried on the cargo hook is the lesser of that specified by the Flight Manual Supplement – Cargo Hook issued by Bell for your particular model or 3500 lbs (1588 kg).



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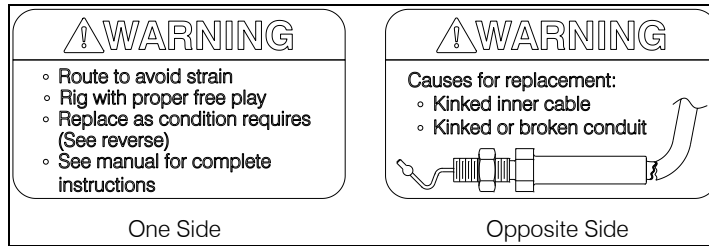
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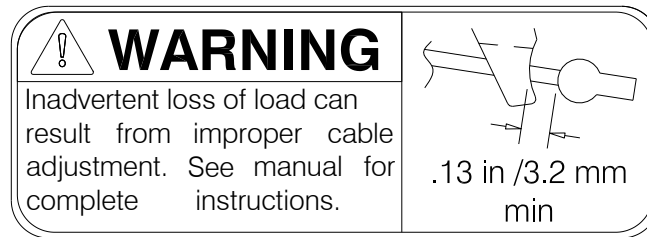
1-20 Placards

The following placards are applicable to the 200-260-00 kit.

- Located on the manual release cable near the cargo hook:



- Located on the bottom of the cargo hook:



2. NORMAL PROCEDURES

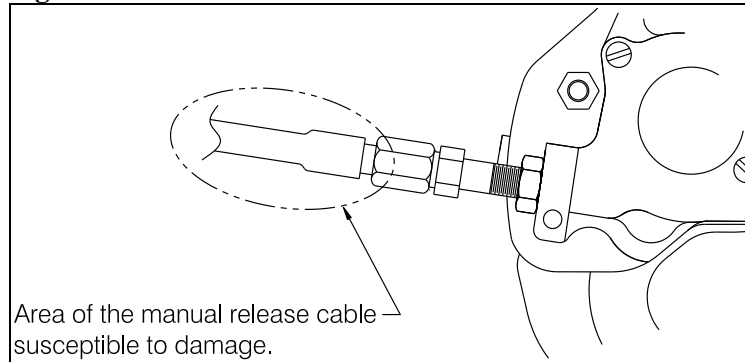
The basic Rotorcraft Flight Manual and Rotorcraft Flight Manual Supplement – Cargo Hook issued by Bell remain applicable and are complemented by the following.

2-3 Pre-flight Check

Before a flight involving external load operations perform the following procedures. If the procedures are not successful do not use the equipment until the problem has been corrected.

1. Check all cargo hook fasteners to ensure that they are tight.
2. Check the cargo hook case and covers for cracks and damage.
3. Check the cargo hook load beam for gouges and cracks.
4. Check the electrical connector for damage and security.
5. Check the manual release cable for damage, paying close attention to the flexible section at the area of transition to the end fitting (see below). In this area, check for splitting of the heat shrink and kinked or broken conduit underneath and any sign of separation from the steel end fitting.

Figure 1 Manual Release Cable Check



2. NORMAL PROCEDURES continued

2-3 Pre-flight Check continued

6. Cycle the manual release mechanism to ensure proper operation. Pull the manual release lever in the cockpit. The cargo hook load beam must open. Return the cargo hook load beam to the locked position by manually pushing up on it. The load beam should snap shut. The cargo hook may be flown in the open position to facilitate loading by a ground crew.

NOTICE

The cargo hook interfaces with the rotorcraft's manual release system as supplied by Bell. Consult the Flight Manual Supplement – Cargo Hook for operation of manual release system.

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2. **NORMAL PROCEDURES** continued

2-3 **Pre-flight Check** continued

7. Cycle the cargo hook's electrical release mechanism to ensure proper operation. Pressing the CARGO RELEASE switch on cyclic should cause the cargo hook load beam to open. The cargo hook may be returned to the locked position by manually pushing up on the load beam. The load beam should snap shut.

NOTICE

The cargo hook interfaces with the rotorcraft's electrical release system as supplied by Bell. Consult the Flight Manual Supplement – Cargo Hook for operation of the electrical release system.

8. Move the cargo hook and the beam assembly throughout their full ranges of motion to ensure the manual release cable and electrical harnesses have enough slack. The cable or electrical harnesses must not be the stops that prevent the cargo hook or beam assembly from moving freely in all directions.



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2. NORMAL PROCEDURES continued

Cargo Hook Rigging

Extreme care must be exercised in rigging a load to the Cargo Hook. The following illustration shows the recommended rigging configuration and rigging configurations to avoid.




The examples shown are not intended to represent all possibilities. It is the responsibility of the operator to ensure the hook will function properly with the rigging.

Nylon Type Straps or Rope

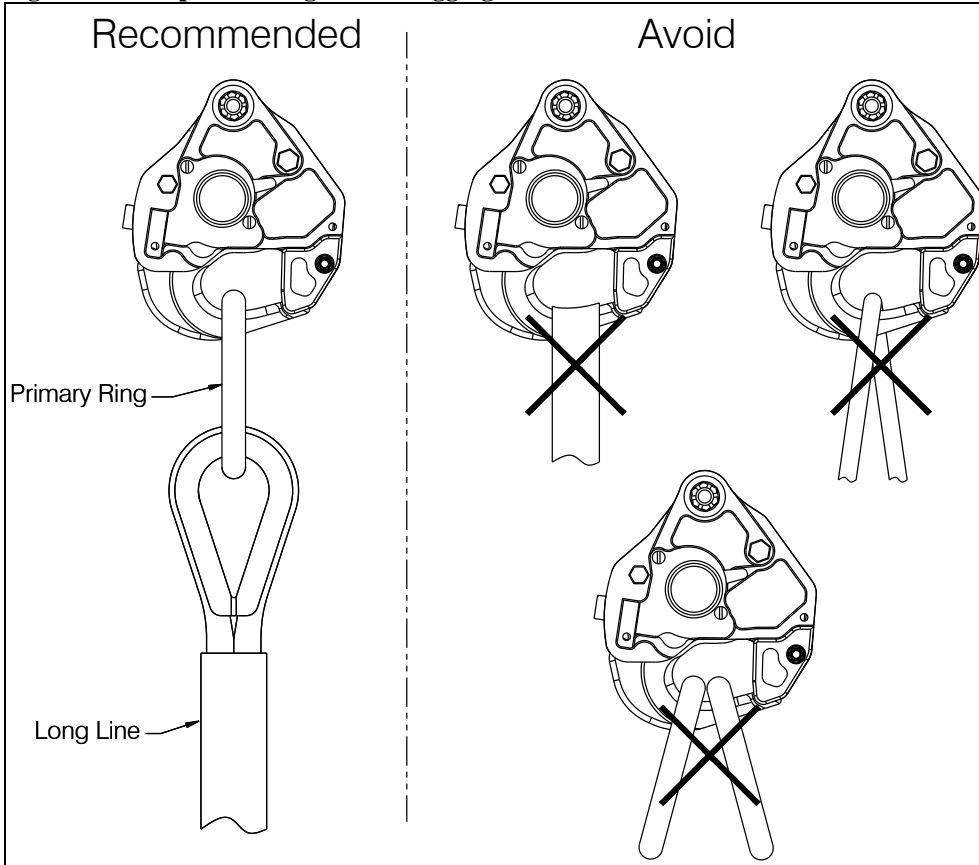


Nylon type straps (or similar material) or rope must not be used directly on the cargo hook load beam. If nylon straps or rope must be used they should be first attached to a steel primary ring. Verify that the ring will freely slide off the load beam when it is opened. Only the primary ring should be in contact with the cargo hook load beam. See Figure 2.

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2. NORMAL PROCEDURES continued

Figure 2 Examples of Cargo Hook Rigging



3. EMERGENCY PROCEDURES

The Rotorcraft Flight Manual Supplement – Cargo Hook issued by Bell remains applicable.

4. PERFORMANCE

The Rotorcraft Flight Manual Supplement – Cargo Hook issued by Bell remains applicable.

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